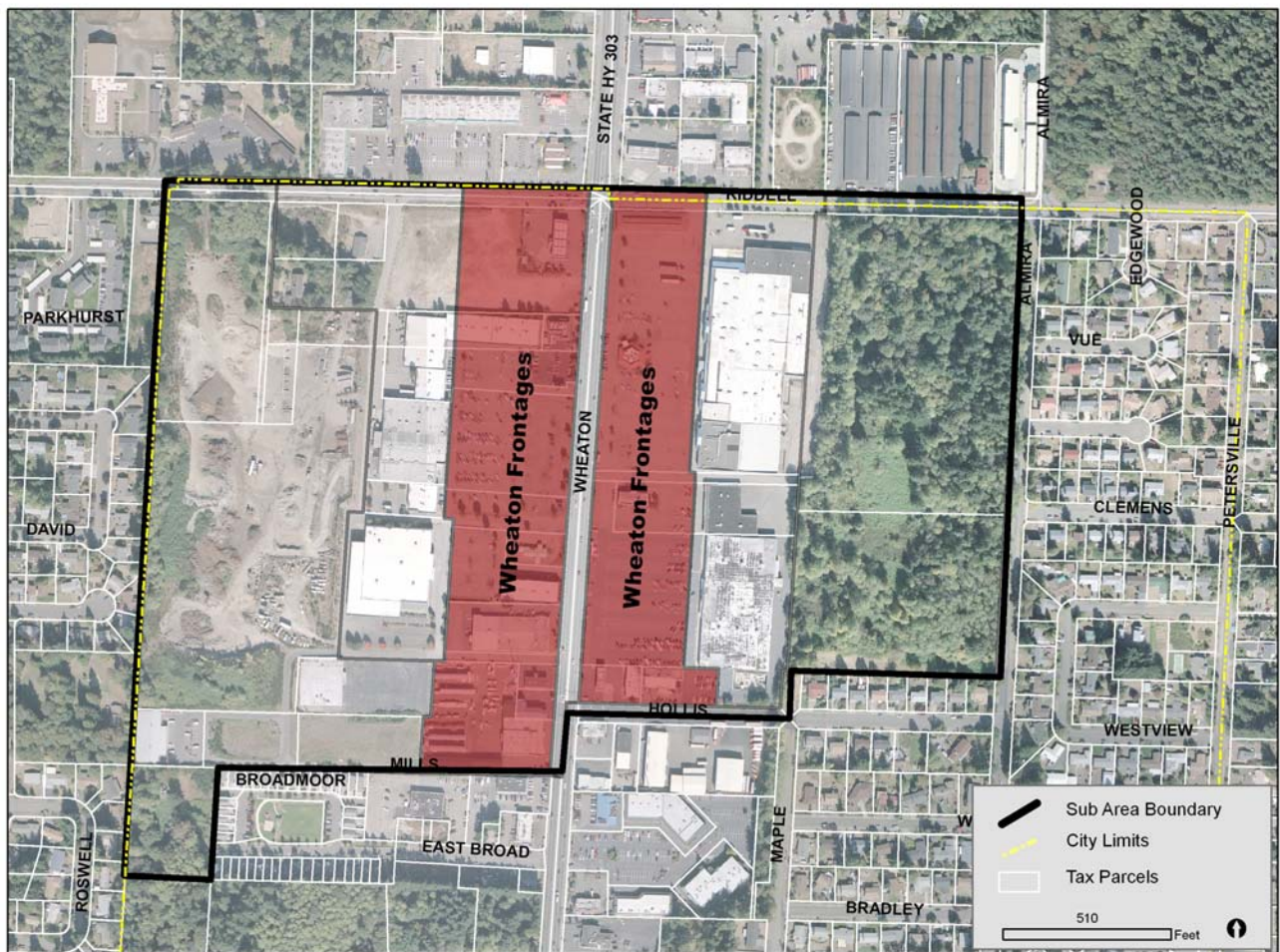


IV.WF - WHEATON FRONTAGES FOCUS AREA

WHEATON FRONTAGES FOCUS AREA

The Wheaton Frontages extend from the Wheaton Way ROW to the east and the west for a depth ranging from 285' to 315'. The interior edges (those furthest from Wheaton Way) follow parcel lines where they are present, and the approximate location of an existing or potential mid-site circulator where no parcel lines are present. (See figure below.) Detailed development standards that will function as zoning within the Wheaton Frontages are also included in this subsection.



PREFERRED URBAN DESIGN

The preferred urban design for the Wheaton Frontages seeks to accommodate the preferred roadway improvements on Wheaton Way (Section III). Existing zoning for the area (District Center Core (DCC)) already fits fairly well with the vision for Wheaton / Riddell Sub Area as expressed in this planning process. Therefore, the DCC designations is used as a starting point and only minor modifications are made. The following are the key concepts for how the DCC designation needs to be modified and refined in order to meet the intent of the overall Sub Area Plan. Most important within this focus area is the need to craft appropriate setbacks and improvement requirements for the front (Wheaton Way) setback. The following principles lay out the urban design strategy for the Wheaton Frontages.

Accommodate Wheaton Way Right of Way (ROW) Improvements:

Wheaton Way will receive improvements over time in a phased manner, beginning with access control, turn pockets and streetscape upgrades within the existing ROW. Later the roadway may be expanded with additional lanes per WSDOT studies. The following objectives seek to integrate land use setbacks with future Wheaton Way improvements:

- *Right of Way Upgrades:* Development in the Wheaton Frontages should contribute to and frame phased improvements to the Wheaton Way corridor. Since Wheaton Way (SR 303) is a state highway, many of the roadway upgrades will be funded by state sources pursuant to the SR 303 improvements. However, development must also contribute to frontage improvements within the edge of the ROW. New development should provide targeted streetscape features that will be added to later during the larger phased WSDOT project.
- *Curb Cut Consolidation:* Curb cuts directly onto Wheaton Way should be consolidated to one main entry / exit at the midpoint of the Center (See Section III). This is a part of the preferred urban design strategy for Wheaton / Riddell as a whole. Curb cut consolidation is also an access control measure that streamlines circulation. Other entries and exits onto Wheaton will be made via the existing signalized intersections at Riddell Rd. and at Hollis St. No new curb cuts onto Wheaton should be allowed, and new development should consolidate existing curb cuts.

Incorporate Frontage Road & Interior Circulation:

The Wheaton Way Improvements detailed in Section III of this Plan call for an interior circulation network, including a neighborhood scaled frontage road.

- *Frontage Road Within Setback:* A calm, neighborhood scale frontage road should be created within the required front setbacks of properties within the Wheaton Frontages. A space allowance, plus parking lot striping should be provided to enable the creation of the frontage road. In the long term, if WSDOT requires ROW expansion for additional travel lanes, the frontage road may be converted to ROW at a future time.
- *Amenity Zone:* In addition to frontage road circulation, an amenity zone adjacent to the base of new infill buildings should be provided. The amenity zone will 'look and feel' like a public sidewalk zone, but it will be located on private property associated with the new development.
- *Mid-Site Circulator:* New development in the Wheaton Frontages will back up to a Mid Site circulator. Development in the Wheaton Frontages should not preclude creation of the mid site circulator. The City should review site design to ensure adequate space is provided to accommodate the mid-site circulator.

Infill Development on Surface Parking Lots:

Long term redevelopment of existing surface parking lots is a critical element of the Sub Area Plan. Redevelopment of surface lots helps to achieve numerous Plan goals such as: activation of the streetscape, densification of development, meeting growth targets, and creating a more human scale to the Wheaton Way. (See Section II). The current land use and zoning scheme (DCC) takes steps to encourage infill on surface lots but it must be modified. Modifications are needed to make sure the unique conditions of Wheaton Way are addressed, and to provide more options for infill development.

- *Flexibility of Building Setbacks:* The DCC zone setback (Maximum 10') is too restrictive for development and does not accommodate a frontage road or Wheaton Way ROW upgrades. The Sub Area Plan proposes a flexible setback range: Minimum 30' and Maximum 55'. Infill buildings should have their front façade within that setback range. Within the setback there are options for varied configurations of frontage road, storefront parking and amenity zone.
- *Buildings Oriented 'Inward' or 'Outward':* Developers placing infill on surface lots may wish to face buildings towards Wheaton Way, or they may wish to face buildings inward to the center of the site. When a building faces outward it should contribute to an attractive Wheaton Way streetscape befitting of a center district. When buildings face inward, their 'backs' facing Wheaton Way must be attractively screened and designed.
- *Storefront Parking:* Retail businesses often prefer to have storefront parking that customers can see from the main roadway. The DCC zone does not allow parking in front of a business on Wheaton Way. Through the Sub Area Plan, some storefront parking should be allowed facing Wheaton Way. With the addition of the frontage road, a row of storefront parking can be added without detracting from the pedestrian experience. A single row of parking can even contribute to a protective buffer for pedestrians.
- *Most Intense Building Scale:* Since the Wheaton Way frontages are adjacent to an arterial street, the area may accommodate a high level of building scale and intensity. The Wheaton Way frontages should allow for buildings up to 65' high, and other limitations on the scale of buildings within the focus area should be minor.

Contribute to Pedestrian and Nonmotorized Circulation Network:

The preferred urban design for Wheaton Way frontages calls for a system of frontage road, storefront parking and amenity zone. Features should be provided within this strategy to ensure a coherent and efficient pedestrian and nonmotorized circulation network.

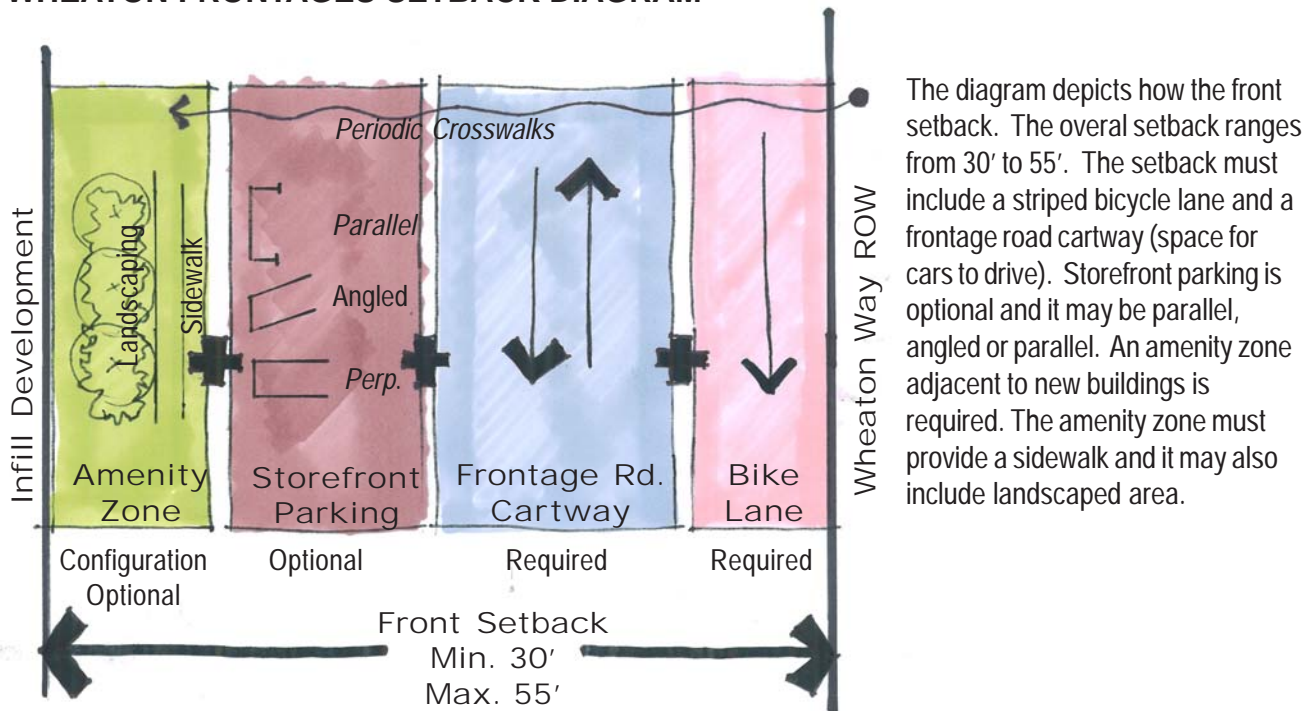
- *Bicycle Lane:* A striped bicycle lane within the front setback adjacent to the Wheaton Right of Way.
- *Amenity Zone Improvements:* Include specific improvement standards for the amenity zone adjacent to new buildings that contribute to pedestrian circulation and connections. This will include standards for 'hardscape', plantings, and building façade treatments.
- *Crosswalks to Amenity Zone:* Since the sidewalk in the public ROW will be located across the frontage road from the amenity zone, crosswalk connections should be provided at intervals to ensure safe pedestrian circulation.
- *Through Corridors:* Where frontage buildings are wide or oriented to the interior, periodic through corridors for pedestrians to reach the site interior should be provided.
- *Contribute to ROW Improvements:* Any new development within the Wheaton Frontages, or in the site's interior (if on a contiguous parcel) should contribute to sidewalk improvements in the public right of way.

WHEATON FRONTAGES CONCEPT

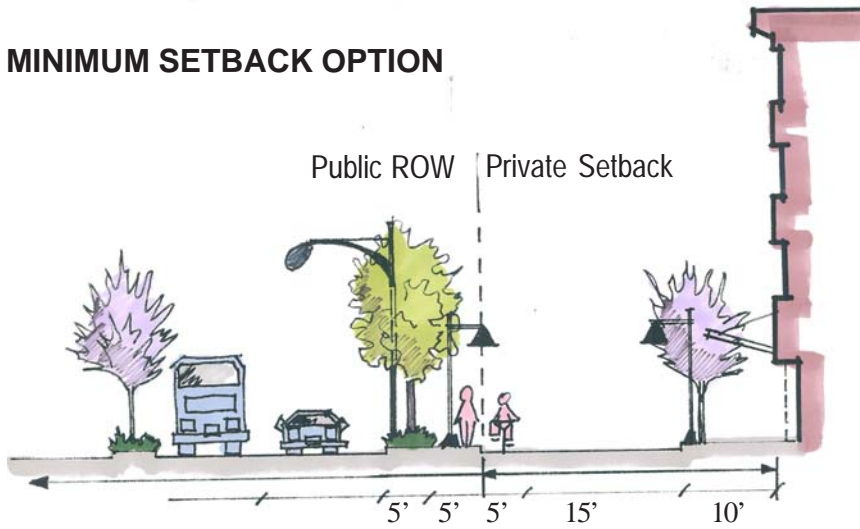


This illustration shows the concept for how Wheaton Way could be reconfigured and a neighborhood scale frontage road could be added. The sidewalk and planting strip at the edge of Wheaton Way would be in the public ROW, and some of the improvements would be a part of the WSDOT phased improvement project. The frontage road, storefront parking and amenity zone adjacent to new building would be located in the front setback. In the long term, if additional lanes on Wheaton Way are required the frontage road and storefront parking could be converted to public ROW.

WHEATON FRONTAGES SETBACK DIAGRAM

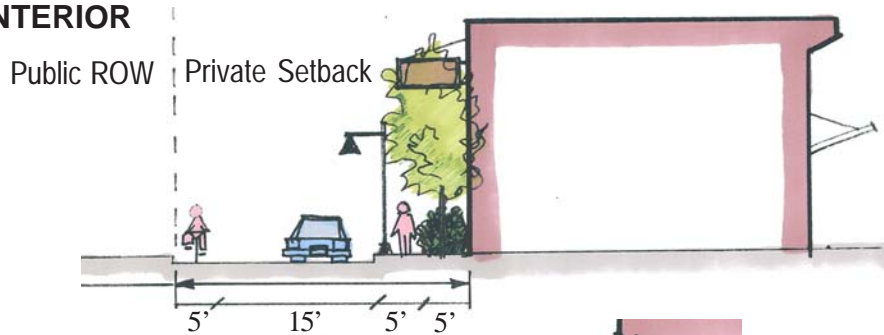


MINIMUM SETBACK OPTION



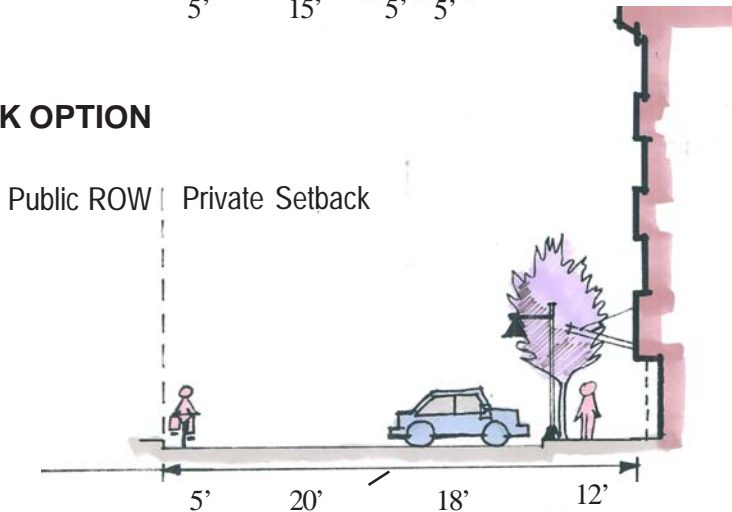
This example uses the minimum setback. There is no storefront parking, the vehicle cartway is 15', a 10' hardscape amenity zone is provided next to the new building, a 5' bicycle lane is provided. Streetscape improvements including pedestrian lighting and plantings are made in the ROW.

**MINIMUM SETBACK OPTION
ORIENTATION TO INTERIOR**



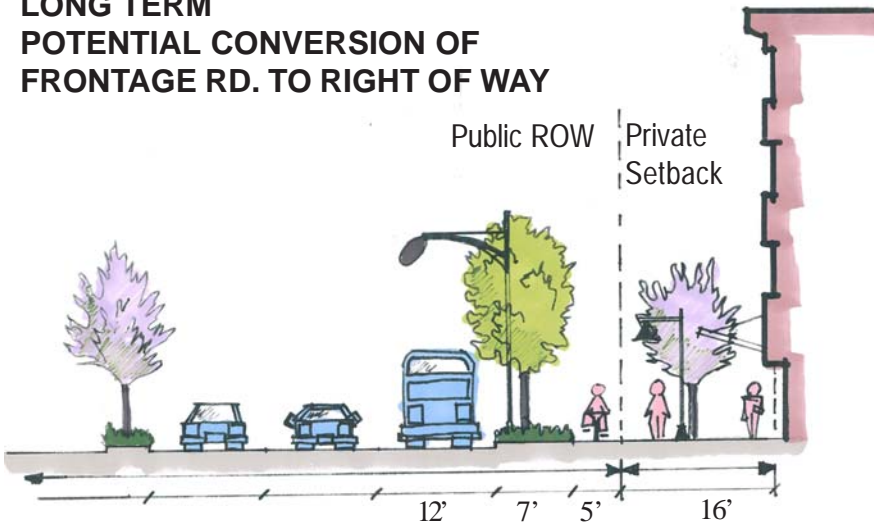
This example also uses the minimum setback, but the building is oriented to the site interior (Not Wheaton Way.) In this case the amenity zone facing Wheaton must include attractive screening.

MAXIMUM SETBACK OPTION



This exmple uses a maximum setback. The vehicle Cartway is the maximum 20', perpendicular storefront parking at 18' is provided, and the amenity zone is 12'.

**LONG TERM
POTENTIAL CONVERSION OF
FRONTAGE RD. TO RIGHT OF WAY**



This exmple shows how the frontage road could be converted to ROW in the long term. The building is in the same position as the first example. Even With ROW expansion an ample amenity zone would remain.

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DEVELOPMENT STANDARDS – WHEATON FRONTAGES (WF)

The following development standards apply to the Wheaton Frontages as zoning for this focus area. The standards below are supplemental to the BMC. Topics not explicitly regulated below remain subject to all provisions of the BMC. Where topics explicitly regulated below diverge from those of the BMC, the development standards below supersede.

A. Outright Permitted Uses

BMC Section 20.70.020 shall apply, except that 20.70.020 (u) is modified to read “Residential uses of all types”.

B. Prohibited Uses

Section 20.70.050 shall apply.

C. Wheaton Way Building Setbacks – (See Also Figures In This Section)

Setbacks from the (2007) Wheaton Way Right of Way shall be as follows:

- (1) Minimum building setback: 30'
- (2) Maximum building setback 55'
- (3) 80% of the building façade facing Wheaton Way must be within the minimum – maximum setback range
- (4) Awnings, canopies, stoops and porches may protrude into the Wheaton Way building setback by 5'

D. Frontage Road & Amenity Zone Requirements – (See Also Concept Figures In This Section)

The applicant is responsible for providing features to create a neighborhood scale frontage road with an amenity zone at the base of new buildings fronting Wheaton Way. The amenity zone and frontage road features must be provided for the full width of the parcel facing Wheaton Way, except that when a development occupies a small portion of the parcel width the required amenity zone improvement may be reduce to only that portion at the base of the new building:

- (1) **Bicycle Lane:** A 5' bicycle lane shall be provided adjacent to the Wheaton Way ROW. On the east side of the District the lane is for northbound travel, on the west side for southbound travel. The bicycle lane must be striped with standard roadway striping techniques and standard symbology for bicycle lanes.
- (2) **Cartway:** A vehicle cartway with a minimum width 15' and a maximum width of 20' for locally circulating two-way traffic shall be provided adjacent to the bicycle lane. This shall function as the frontage road.
- (3) **Storefront Parking:** A single row of parking may be, but is not required to be, located within the Wheaton Way building setback adjacent to the Cartway. Parking may be perpendicular, angled, or parallel and shall have a maximum depth of 18'.
- (4) **Amenity Zone:** The amenity zone may be expanded beyond 10' in width by any amount, provided the maximum building setback is met.
 - a. **Sidewalk:** At least 5' of the amenity zone's width must be sidewalk. Sidewalk may be concrete, or decorative paver materials. Sidewalk may not be non-decorative asphalt paving.
 - b. **Plantings:** All of the amenity zone not comprised by sidewalk shall be attractively planted per the landscape requirements below.
- (5) **Amenity Zone Expansion:** For every foot of width beyond 10' the amenity zone is expanded, the maximum building setback may be increased 1 foot, to a maximum increase of 10'.
- (6) **Pedestrian Lighting:** Pedestrian scale lighting of the amenity zone shall be provided with fixtures at intervals not less than 25'. Pedestrian lighting may be achieved with streetlamps, lit bollards, building mounted lamps, or other similar pedestrian oriented lighting techniques. All pedestrian lighting in the district shall have high cut-off fixtures.
- (7) **Pedestrian Connection:** Crosswalk connections from the Wheaton ROW across the frontage road to the amenity zone shall be provided at intervals not exceeding 250'. Crosswalks must be constructed of a material with different texture or color from the surface of the frontage road. Allowable techniques include gridded or colored concrete, or textured pavers.

E. Parking

- (1) **Parking Quantity:** Parking quantities are per District Center Core zone 20.70.080 (a), (b) and (g).

- (2) **Storefront Parking:** Parallel, angled or perpendicular Storefront Parking located adjacent to the Frontage Road shall count toward the parking requirement.

F. Development Standards

- (1) Maximum Building Height: 65'
- (2) Maximum Residential Density: None
- (3) Side & Rear Setbacks: No minimum or maximum, provided requirements for the mid-site circulator and secondary façade screening area met.
- (4) Building Coverage: No minimum or maximum, provided frontage road, amenity zone, design and landscaping standards are met.
- (5) Development Coverage: No minimum or maximum provided frontage road, amenity zone, design and landscaping standards are met.

G. Design Standards

New development must include design features to contribute to the public realm and the overall unity and vitality of the Center. Because buildings may be oriented to either Wheaton Way or to the site interior, a building must have at least one designated Front Façade that meets the full range of design standards befitting of a Center. All other building facades must meet standards for Secondary Facades that also contribute to the Center environment and pedestrian experience, but are not as stringent. Any building may be designed with multiple Front Facades.

(1) Front Façades:

- a. **Window Fenestration:** For ground floor commercial uses, large windows with transparency allowing views into interior spaces or onto displays must comprise 65% or more of the façade between the height of 2' and 10' above sidewalk grade.
- b. **Ground Floor Residential:** For ground floor residential uses, ground-related housing units are required, and an entry feature to each ground related unit such as a porch or stoop must be included.
- c. **Building Entrances:** Commercial building entries shall be easily identifiable from the street and sidewalk. This may be accomplished by: a recess or protrusion of at least 3'; or a significant architectural detail or feature around the door.
- d. **Awnings and Canopies:** For commercial structures an overhang awning or canopy shall be provided for at least 40% of the building's Front Façade width between the height of 8' and 20' above grade.

(2) Secondary Facades:

- a. **Blank Facades:** No Secondary Façade may be blank. Secondary facades must be variegated with a combination of changes in plane, changes in color and material, windows or architectural features, decorative signage, or other devices to add interest. Variegating features must be included at horizontal or vertical intervals not exceeding 20 feet.
- b. **Landscape Screening:** Landscape screening must be placed at the base of all secondary facades per the landscape requirements below.
- c. **Facing Wheaton Way:** Where a secondary façade faces Wheaton Way either: a pedestrian through corridor 10' in width from the Amenity Zone to the interior of the site; or a public building entry, must be located at horizontal intervals not exceeding 125'.

(3) North / South Oriented Buildings

- a. When a building is oriented in a north / south direction so that longer facades face north and south, and a short façade faces Wheaton Way, the City shall apply Amenity Zone and Front Façade Standards to one north / south facing façade. This is in addition to the amenity zone requirements for the Wheaton Way fronting façade.

H. Landscape Requirements

Landscaping requirements are per BMC Chapter 20.50 with the following exceptions.

- (1) **Street Trees:** Street trees shall be planted as follows. Averaging may be used to allow clustering.
 - a. **In Amenity Zone:** One tree of DBH 2.5 inches each 25' on center.

- b. **In Wheaton ROW:** One tree of DBH 2.5 inches or greater each 25' on center.
- (2) **Amenity Zone:** Those portions of the Amenity Zone not comprised of sidewalk shall be planted as follows:
 - a. Shrubs, flowers, natural grasses, ferns, hedges, or similar thick vegetative covering shall comprise at least 60% of the area. Turf grasses (lawn) do not fulfill this requirement, and native vegetation is preferred.
 - b. If drought resistant landscape treatments are used, the Department may reduce the vegetative coverage requirement below 60%.
- (3) **Secondary Façade Screening:** Secondary facades shall be landscaped as follows. Where a secondary façade faces Wheaton Way, these standards supersede H(2) above.
 - a. **Landscaped Area:** A landscaped area at least 5' in width at the base of the secondary façade shall be planted with shrubs, flowers, natural grasses, ferns, hedges or similar thick vegetation. Vegetation must cover at least 80% of the area.
 - b. **Planting Screen:** A thick vegetative sight barrier a minimum of 5' high shall be located within the landscaped area for the width of the secondary façade.

I. Mid-Site Circulator

New construction must not preclude the creation of a principal north / south mid-site circulation road. The mid-site circulator roadway shall be located in the general vicinity as indicated on the circulation figure (Section I) at the interface between the Wheaton Way frontage, and Site Interior zones. The mid-site circulator shall provide a vehicle cartway not less than 24' wide. The City may perform site plan review to ensure that location of buildings do not preclude the creation of a mid-site circulator. The mid-site circulator shall enable connections to the mid-site circulator on adjacent parcels.

J. Ownership & Construction Standards

Both the frontage road, and the mid-site circulator are located on private property. The following construction and maintenance standards apply in addition to those of the BMC engineering division.

- (1) **Construction:** The frontage road and mid-site circulator are not required to be constructed to standards of a City roadway. The surfaces of the frontage road and mid-site circulator must be constructed to the standard of a commercial parking lot.
- (2) **Striping:** Striping of the bicycle lane, the frontage road, and the mid-site circulator shall be per the methods and standards of a City roadway. Striping shall demarcate clear circulation and shall be of a readily visible and durable material. The City may require periodic maintenance of the striping to ensure safe and efficient circulation.

K. Signage

Standards of BMC Chapter 20.52 shall apply as follows:

- (1) Downtown and Centers Sign Regulations per (BMC 20.52.140) shall apply to the Wheaton Frontages focus area.
- (2) For the Wheaton Frontages area, BMC 20.52.100 Shopping Center or Professional Office Complex Signs shall apply with the modification that such Complex Sign to a maximum size of 400 square feet is allowed within each half (as divided by Wheaton Way) of the Center. The Complex Sign must be located at the central focus intersection as denoted within this Sub Area Plan.

L. Wheaton Right of Way Improvements (See Also Section II Wheaton Way Corridor)

The following standards, in addition to the requirements of the Bremerton Engineering division shall be applied in conjunction with new development in the Wheaton Frontages focus area. These standards are intended to accommodate and mesh with the Wheaton Way Corridor improvements articulated in this Sub Area Plan.

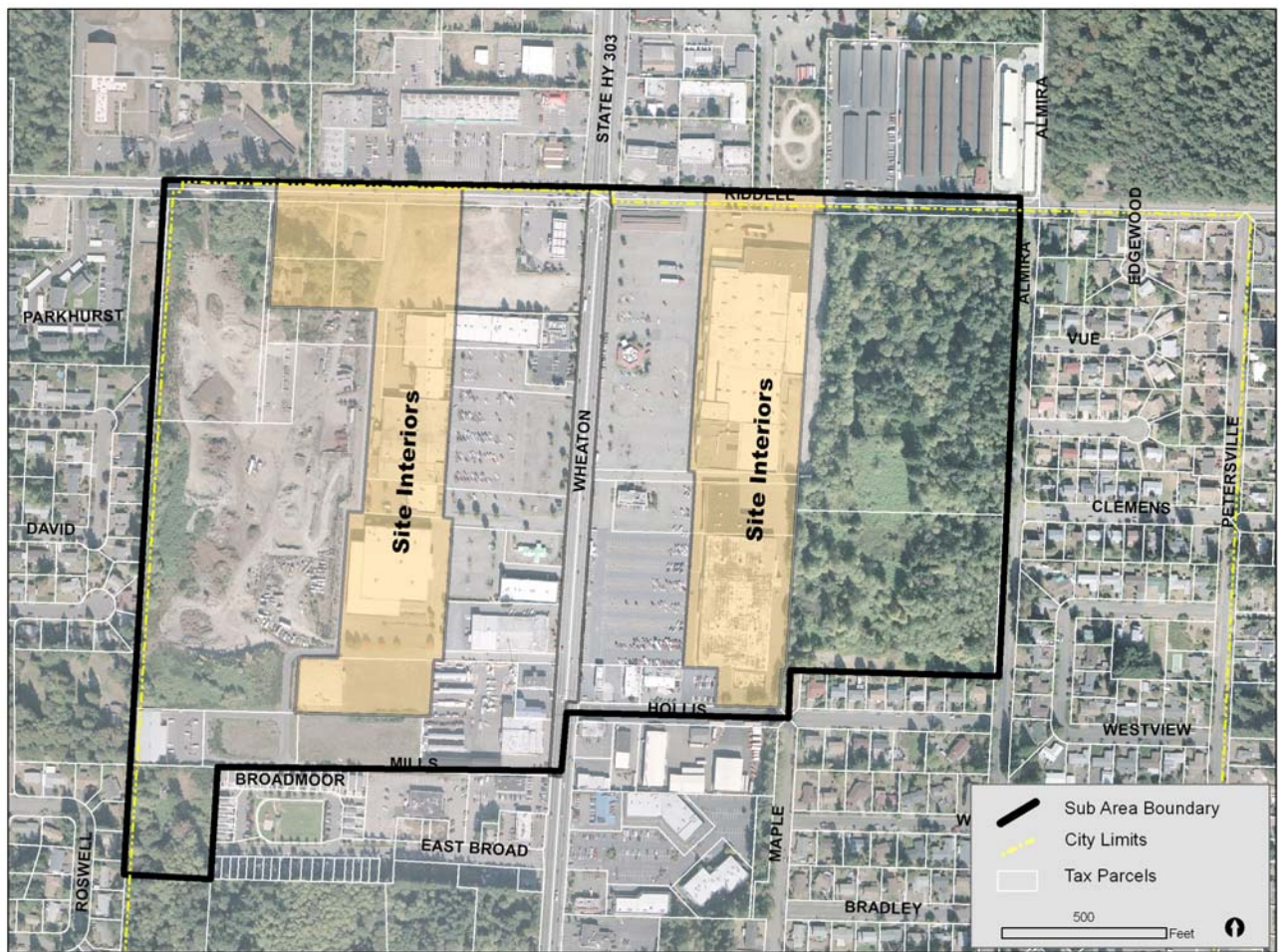
- (1) **Applicability:** Requirements to upgrade the Wheaton Way streetscape shall be applied to any new development adjacent to Wheaton Way, except that when an existing development is located between the project proposal and Wheaton Way, the Department may deem Wheaton Way ROW Improvements not applicable.
- (2) **Configuration:** Wheaton ROW Improvements shall include the following:
 - a. Curb cuts shall be consolidated per this Sub Area Plan.
 - b. A landscaped planter strip not less than 5' in width shall be provided immediately adjacent to the roadway. The planter strip must be densely planted with vegetation comprising not less than 80% of surface area. Turf grasses do not count towards the planting requirement.

- c. A sidewalk not less than 5' shall be provided adjacent to the planter strip.
- c. Pedestrian scaled street lamps shall be provided within the sidewalk or planter area at intervals not less than 50 feet. The Department may condition the selection of streetlamp model, to ensure durability and/or to promote a consistent style throughout the District Center.
- d. Street trees per the Landscape Requirements (H) above shall be provided.

IV.SI - SITE INTERIORS FOCUS AREA

SITE INTERIORS FOCUS AREA

The Site Interiors Focus Area extends from the back edge of the Mid Site Circulator. The Site Interiors begin at a setback ranging from roughly 285' to 315' from Wheaton Way as shown below. This is the approximate location of the mid-site circulator as depicted in Section II of this Sub Area Plan and the front of the existing box stores. The Site Interiors focus area extends on the west to the edge of City Villa, and to the east, the edge of the current forested area.



PREFERRED URBAN DESIGN

The preferred urban design for the Site Interiors allows for intensive infill and refill development and accommodates an improved interior circulation network. The existing District Center Core (DCC) designation is used as a starting point and only minor modifications are made. The following are the key concepts for how the DCC designation needs to be modified and refined in order to meet the intent of the overall Sub Area Plan. Most important within the focus area is the need to accommodate interior circulation.

Accommodate Interior Circulation:

The sub area plan calls for a network of circulation routes to make a more efficient and orderly overall circulation network. The Site Interiors are framed by the proposed interior circulator routes.

- *Accommodate Mid-Site Circulators:* The Sub Area Plan calls for mid-site circulators in a north / south direction between the Wheaton Way Frontages and the Site Interiors focus areas. New Development must not be located to preclude the mid-site circulator. The Mid-Site circulator can be dedicated as a public ROW or it can remain a private driveway. Mid-Site Circulators should connect to adjacent properties and Riddell Rd. at the north and to Hollis St. at the south where possible. Wherever possible the Mid-Site Circulator should be a continuous straight drivelane.
- *Accommodate Site Interior Circulators:* The Sub Area Plan calls for site interior circulators in a north / south direction at the back edges of this focus area. New Development must not be located to preclude the Site Interior circulator. The Site Interior circulator can be dedicated as a public ROW or it can remain a private driveway across properties. The Site Interior circulator should connect to adjacent properties and Riddell Rd. at the north and where possible to Hollis St. at the south. Wherever possible the Mid-Site Circulator should be a continuous straight drivelane.
- *Street Treatments of Circulators:* Some streetscape and roadway elements should be included along the Mid-Site Circulators and Site-Interior Circulators to give them the quality of attractive roadways. Parking lot plantings should be located to frame the edges of the circulators. Striping can be used to clearly identify the circulators. Where possible curb, and sidewalk elements can be added to make the circulators function more like streets.

Infill & Refill Development:

Much of the focus area contains existing buildings, but there is opportunity for infill redevelopment of. Density including construction of new, mixed use and multiple story building in the area, or addition of upper stories to existing buildings is encouraged.

- *Building Scale:* Since the Wheaton Way frontages are in an interior site location, and area generally buffered from small scale residential neighborhoods the area may accommodate a high level of building scale and intensity. The Wheaton Way frontages should allow for buildings up to 55' high. Other limitations on the scale of buildings within the focus area should be minor to allow flexibility in redevelopment.
- *Flexibility of Building Setbacks:* Setbacks in the Site Interiors are flexible. Buildings should be located with one frontage along either the Mid-Site or the Site-Interior Circulator. A maximum setback from a designated frontage is 40'. (See also Quality Building Design Below).

Quality Building Design & Street Activation

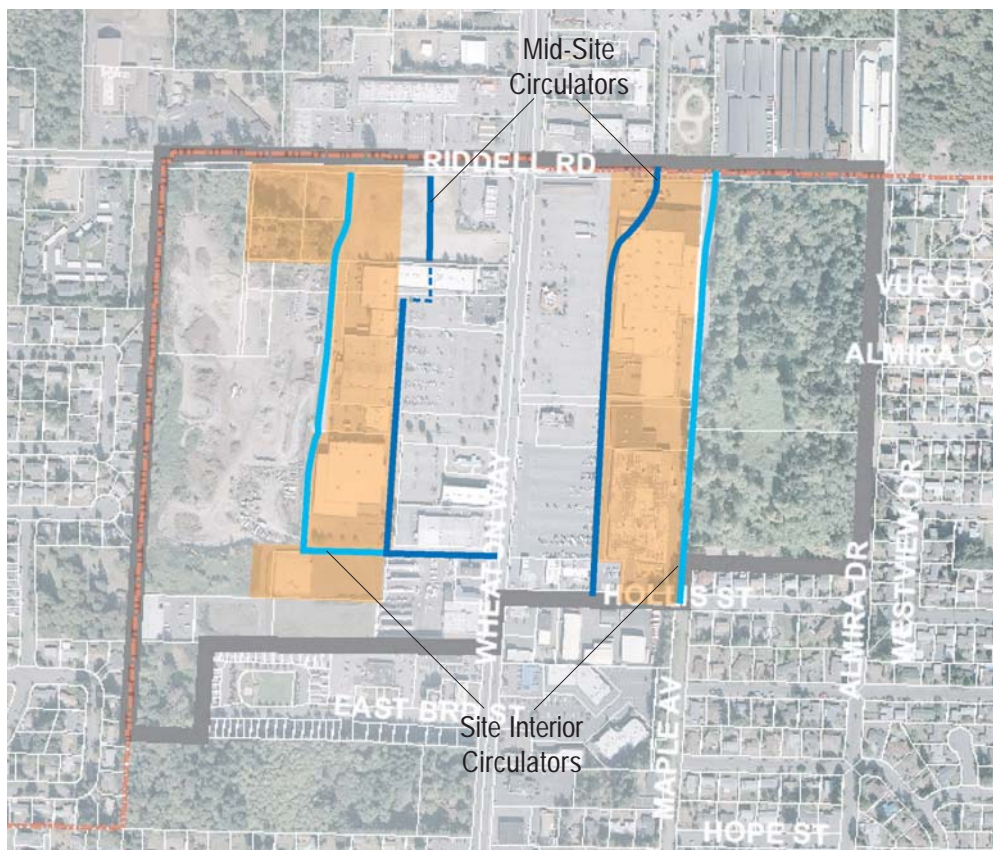
New development must have building form and design quality befitting of a center. New development should help activate the streetscape along streets and private circulators, and add to the overall harmony of the District Center.

- **Building Orientation and Citing:** Developers placing infill or redevelopment in this area may wish to face buildings towards either the Mid-Site Circulator, or they may wish to face buildings towards the Site Interior Circulator. Buildings must have one frontage within 40' of either of the circulators to contribute to an enclosed street environment.
- **Façade Treatments:** New development should have at least one attractive primary front façade treatment. No façade should be blank or unscreened as it faces a public ROW or private circulator.

Pedestrian and Non-motorized Connections

Features should be provided within this strategy to ensure a coherent and efficient pedestrian and nonmotorized circulation network.

- **Contribute to ROW Improvements:** Any new development within the Site Interiors Focus area that does not have infill development between it and Wheaton Way should contribute to Wheaton Way Right of Way improvements.



Redevelopment in the Site Interiors area should reserve space for interior circulation via the mid-site circulators and the site interior circulators.

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SI - SITE INTERIORS DEVELOPMENT STANDARDS

The following development standards apply to the Site Interiors as zoning for this focus area. The standards below are supplemental to the BMC. Topics not explicitly regulated below remain subject to all provisions of the BMC. Where topics explicitly regulated below diverge from those of the BMC, the development standards below supersede.

A. Outright Permitted Uses

Section 20.70.020 shall apply, except that 20.70.020 (u) is modified to read "Residential uses of all types".

B. Prohibited Uses

Section 20.70.050 shall apply.

C. Building Frontage

All setbacks in the zone are from the edges of the Site Interiors Focus Area.

(1) **Maximum Building Setback:** 40' from either the Mid-Site Circulator or the Site Interior Circulator at the applicant's discretion provided:

- a. No more than one row of storefront parking is located within the Maximum Building Setback.
- b. 80% of the building façade must meet the maximum building setback.
- b. Where a secondary façade is positioned to meet the maximum building setback, all screening requirements per H(2) of this section must be met.

D. Improvements to Circulator Routes

The applicant is responsible for accommodating the Mid-Site Circulator and the Site Interior Circulators as depicted in this sub area plan.

- (1) **Circulators:** Development may not be located to preclude the citing of the Mid-Site Circulator or the Site Interior Circulator. A vehicle cartway of at least 22 feet shall be maintained for both circulators. Wherever possible circulators shall be continuous and straight roadways with connections onto adjacent city ROWs or private properties. When dedicated as public ROW, the roadway shall meet City of Bremerton engineering standards for construction. Where maintained as a private roadway, the surface shall meet applicable standards for commercial parking lot construction.
- (2) **Continuous Walking Surface:** With new development, one continuous north / south walking surface shall be provided at both the Mid-Site Circulator, and the Interior Circulator. The continuous walking surface is not required to be immediately adjacent to the circulator if it is provided at the base of a building façade. The continuous walking surface may be concrete or decorative paver materials, but may not be plain asphalt.

E. Parking & Loading

- (1) **Parking Quantity:** Parking quantities are per District Center Core zone 20.70.080 (a), (b) and (g).
- (2) **Storefront Parking:** Up to one row of parallel, angled or perpendicular storefront parking may be located in the maximum building setback.
- (3) **Loading:** Loading areas and docks may be located within maximum building setback. All loading areas must be screened per the landscaping requirements in H of this section.
- (4) **Parking Location:** On street parking, storefront parking, and parking associated with a development within the Site Interiors but located within another Focus Area count towards the parking requirement.

F. Development Standards

- (1) Maximum Height: 55'
- (2) Maximum Residential Density: None
- (3) Side & Rear Setbacks: No minimum or maximum, provided requirements for the Circulator and landscaping, and building frontage setback are met.

- (4) Building Coverage: No minimum or maximum, provided setbacks, design and landscaping standards are met.
- (5) Development Coverage: No minimum or maximum provided setbacks, design and landscaping standards are met.

G. Design Standards

New development must include design features to contribute to the public realm and the overall unity and vitality of the Center. A building must have at least one designated Front Façade that meets the full range of design standards befitting of a Center. All other building facades must meet standards for Secondary Facades that also contribute to the Center environment, but are not as stringent. Any building may be designed with multiple Front Facades.

(1) Front Façades:

- a. **Window Fenestration:** For ground floor commercial uses, large windows with transparency allowing views into interior spaces or onto displays must comprise 65% or more of the façade between the height of 2' and 10' above sidewalk grade.
- b. **Ground Floor Residential:** For ground floor residential uses, ground-related housing units are required, and an entry feature to each ground related unit such as a porch or stoop must be included.
- c. **Building Entrances:** Commercial building entries shall be easily identifiable from the street or circulator driveway. This may be accomplished by: a recess or protrusion of at least 3'; or a significant architectural detail or feature around the door.
- d. **Awnings and Canopies:** For commercial structures an overhang awning or canopy shall be provided for at least 40% of the building's Front Façade width between the height of 8' and 20' above grade.

(2) Secondary Facades:

- a. **Blank Facades:** No Secondary Façade may be blank. Secondary facades must be variegated with a combination of changes in plane, changes in color and material, windows or architectural features, decorative signage, or other devices to add interest. Variegating features must be included at horizontal or vertical intervals not exceeding 20 feet.
- b. **Landscape Screening:** Landscape screening must be placed at the base of all secondary facades per the landscape requirements below except where loading areas are located.
- c. **Loading Areas:** Where a secondary façade includes loading areas or docks, landscape screening is required per the standards below, but is not required to be at the base of the building.

(3) North / South Oriented Buildings

- a. When a building is oriented in a north / south direction so that longer facades face north and south, and a short façade faces either of the Circulator roadways, one of the north south facades must meet front façade standards.

H. Landscape Requirements

Landscaping requirements are per BMC Chapter 20.50 with the following exceptions.

- (1) **Street Trees:** Street trees shall be planted as follows along both the Interior and Mid-Site Circulators. One tree of DBH 2.5 inches each 25' on center. Trees located within parking lot 'island' planters may count towards the street tree requirement provided they are adjacent to the Circulator.
- (2) **Secondary Façade Screening:** Secondary facades shall be landscaped as follows.
 - a. **Landscaped Area:** A landscaped area at least 5' in width at the base of the secondary façade shall be planted with shrubs, flowers, natural grasses, ferns, hedges or similar thick vegetation. Vegetation must cover at least 80% of the area.
 - b. **Planting Screen:** A thick vegetative sight barrier a minimum of 5' high shall be located within the landscaped area for the width of the secondary façade.

- c. **Loading Areas:** Where a loading area or dock is located on a secondary façade, the landscape requirements per (2)a and (2)b above may be removed from the base of the building.

J. Ownership & Construction Standards

When the Mid-Site and Interior Circulator roads are maintained as private driveways the following construction and maintenance standards apply in addition to those of the BMC engineering division. When roadways are dedicated as ROW, they shall meet BMC engineering standards for roadway construction.

- (1) **Construction:** The surfaces of the Mid-Site and Interior circulator must be constructed to the standard of a parking lot when not dedicated as ROW.
- (2) **Striping:** Clear and permanent striping to indicate the route of the Circulators shall be per the methods and standards of a City roadway. Striping shall demarcate clear circulation and shall be of a readily visible and durable material. The City may require periodic maintenance of the striping to ensure safe and efficient circulation.

K. Signage

Standards of BMC Chapter 20.52 shall apply as follows:

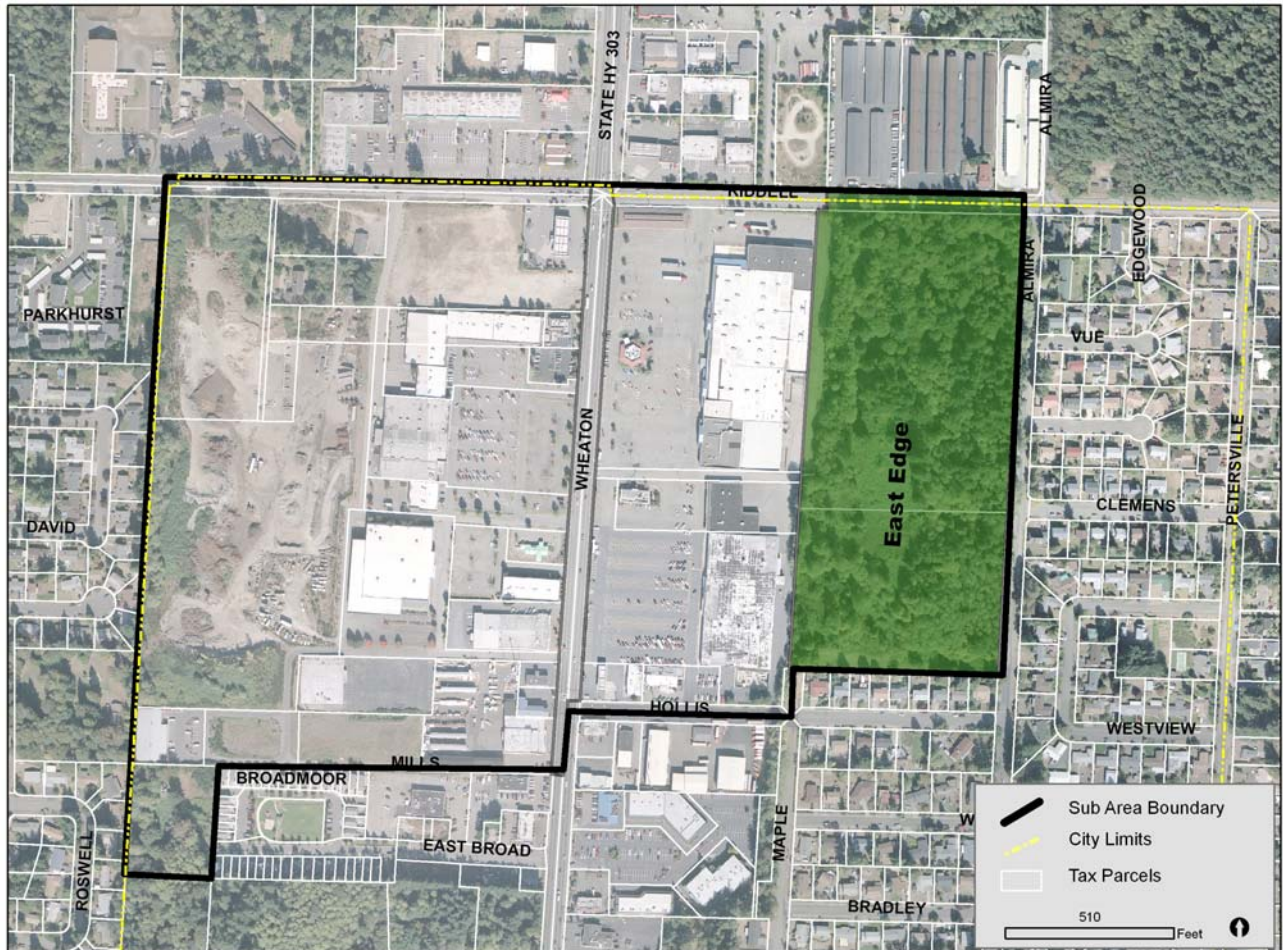
- (1) Downtown and Centers Sign Regulations per (BMC 20.52.140) shall apply to the Site Interiors focus area.

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IV.EE - East Edge Focus

EAST EDGE FOCUS AREA

The East Edge Focus Area extends from the back edge of the Site Interiors to the Sub Area Plan boundary at Almira Drive. The East Edge is clearly identifiable from the aerial photographs. It is a undeveloped forested area and includes just two tax parcels, currently with the same owner, comprising a total of about 20.2 acres.



PREFERRED URBAN DESIGN

The preferred urban design for the East Edge area seeks to meet goals for both infill density, and preservation of natural features and greenery. Both of these concepts were supported strongly during the Sub Area planning process, and are emphasized in the Comprehensive Plan. Both goals can be achieved through site design that emphasizes clustering. The following principles structure the preferred design for the East Edge. Currently, an interim zoning designation of R-10 (Low Density Residential) is placed on the area. The proposed urban design and development standards for the East Edge are significant departures from R-10. Even with these changes, the City recognizes that at a future time owners of the East Edge parcels may wish to partner with the City on a more detailed design for this focus area. If so, the City would work with owners to amend this subsection of the Sub Area Plan.

Green Space Preservation

The sub area plan calls for greening and re-greening of the area.

- *East Edge Forest Buffer:* Development should be clustered away from a substantial forest buffer at the east edge of the site. An average buffer of 85' is proposed. This buffer size protects the steepest topography, retains the leafy green character of Almira Dr., and protects the adjacent single family neighborhood from the impacts of intense land uses in the center, in keeping with Comprehensive Plan goals and policies. Developers may also use averaging and tree preservation tools for flexibility in reducing the size of the buffer.
- *Green Gathering Area:* The Sub Area Plan and the Comprehensive Plan call for a sizeable green gathering area within this half of the District. A green area that preserves natural features and reconnects with the history of Clare's Marsh is proposed. For consistency with goals for neighborhood scale park spaces, the minimum size of the green area is proposed at 1 acre.
- *Flexibility:* Tools to allow a developer flexibility in site design while still meeting base requirements for green space preservation should be included.

Linking Trails

A system of connecting public pathways and trails is called for in the Sub Area Plan and the Comprehensive Plan.

- *Illahee Preserve Link:* A pathway, preferably through the forest buffer, which provides a public linkage to the vicinity of the Illahee preserve should be included. This meets City and County goals for public access to a network of green spaces.
- *Clemens St. Link:* A public pathway, which may include sidewalk and/or more natural trails, should link from the Site Interior portion of the District, through the Green Gathering area and make a connection to Clemens St. This allows for a pedestrian connection to Clemens St., without introducing cut-through vehicle traffic to the neighborhood.

Clustered Infill Density

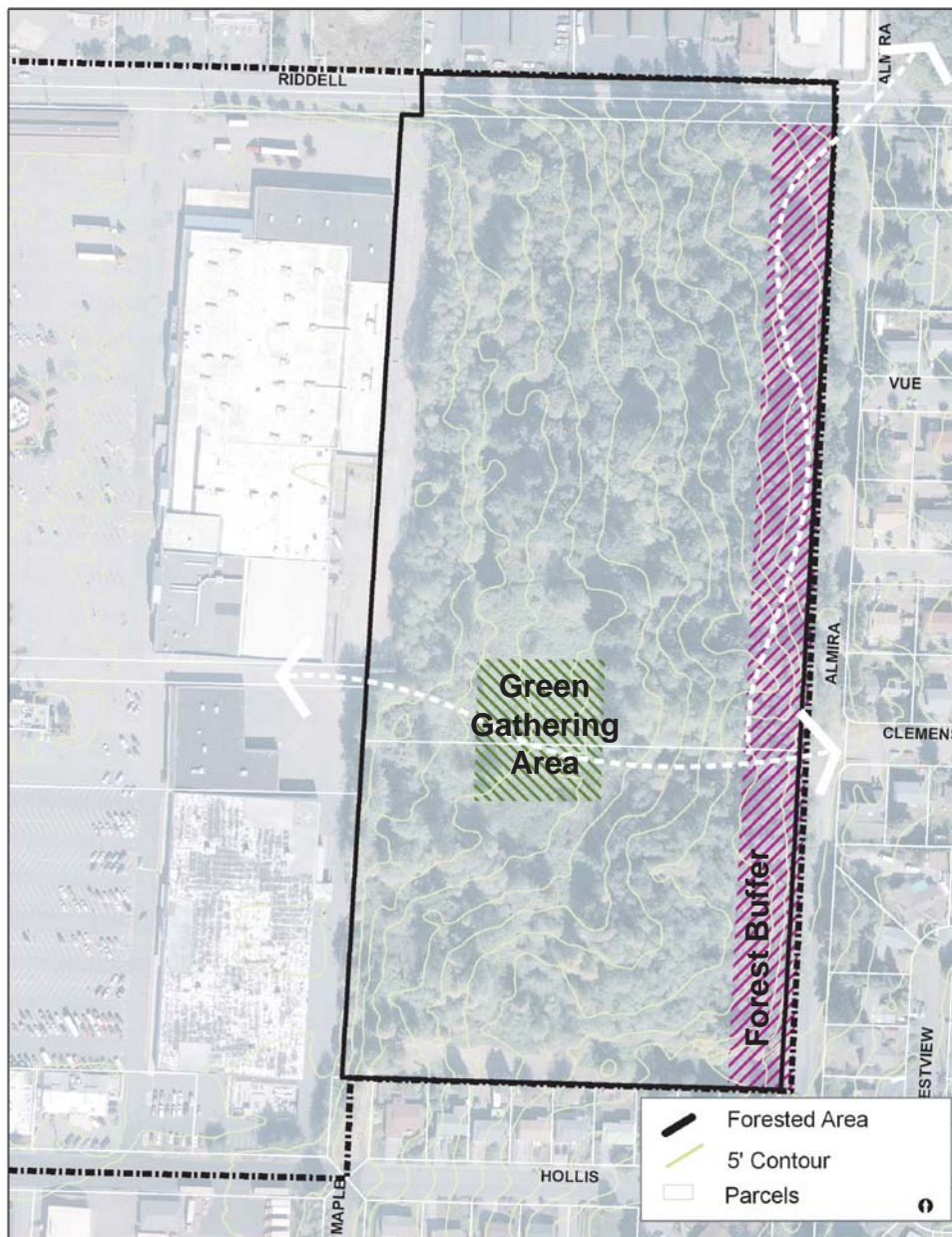
Substantial infill density should be allowed within the East Edge to meet growth goals and add vitality to the District. Such infill density can be achieved, while natural features are preserved if clustering techniques are used.

- *Residential Density:* A clustered residential density equal to 20 units per acre is proposed. The allowable density should be applied to total gross site area before buffer and critical area deductions.
- *Commercial Development:* Commercial development consistent with allowable forms of commercial development in a center District should be allowed, provided buffers and other requirements for green space are met.
- *Ample Zoning Envelope:* To achieve clustered density, an ample zoning envelope must be allowed. Since roughly 30' of topography and a sizeable forest buffer will separate the Almira Dr. residential neighborhood, an ample height limit of 55' will not have neighborhood impacts. Other limitations on the form of development should be minimized.

- *Parking Reductions:* To achieve the desired amount of clustered density, site area must be used efficiently. To reduce the amount of area needed for building footprints, and to reduce cost of construction, techniques to reduce the amount of parking are proposed.
- *Hollis St. Alley:* Ensure that access to the alley at the rear of existing single family homes on Hollis St. is maintained. Provide continue ability by residents to use alley from Maple, provided alley does not infringe on usable area of adjacent property.

Quality Building Design & Street Activation

Standards for a high quality of building design and street activation as consistent with other subsections of this sub area plan should be applied to the East Edge.



The Sub Area Plan calls for a forest buffer at Almira Dr., a public green gathering area, and a system of linking trail connections. If these are provided, a developer is allowed ample clustered density on the remainder portions of the site.

The configuration of the buffer, gathering area and trails are subject to site design. The figure at left depicts them with the minimum sizes as called for in this Sub Area Plan.

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DEVELOPMENT STANDARDS – EAST EDGE (EE) FOCUS AREA

The following development standards apply to the East Edge as zoning for this focus area. The standards below are supplemental to the BMC. Topics not explicitly regulated below remain subject to all provisions of the BMC. Where topics explicitly regulated below diverge from those of the BMC, the development standards below supersede.

A. Applicability

The Development Standards stipulated per C - L of this subsection shall apply only if a proposal meets the criteria per A(1) and A(2) below. If a proposal in the East Edge does not meet A(1) and A(2) criteria, development standards per BMC Chapter 20.60 Low Density Residential shall apply.

- (1) **Forest Buffer Preservation:** Retain a forested buffer along the length of adjacent residential street, Almira Drive with an average depth of 85'. Buffer may meander to include wider portions where slopes extend further into the site, and narrower portions where slopes are less severe. Preserve existing vegetation and trees within the buffer, and execute no substantial grading. Forested buffer must be dedicated as an open space tract, or transferred to ownership by a public Parks & Recreation agency.
- (2) **Public Features:** Provide the following public features in addition to the Forest Buffer:
 - (a) *Green Gathering Area:* Provide a natural green space accessible to the public for gathering in the center of the forested parcel as depicted within this Sub Area Plan. Maintain and enhance natural features such as streams, wetlands, or the large rock located in the vicinity. Provide furniture for use by the public including benches, picnic tables, and/or child's play equipment. Total size of the green gathering area must be at least one acre. Undeveloped critical areas found in this vicinity of the site may contribute to the size requirement for the Green Gathering Area. The Green Gathering area must be dedicated as an open space tract, or transferred to ownership by a public Parks & Recreation agency.
 - (b) *Trail Connections:* Provide a pathway or walkway open to the public connecting at a minimum: Riddell Rd. providing linkage to the Illahee Preserve; the Clemens St. terminus; and the Green Gathering Area. The trail connection may be an improved walkway constructed of concrete or asphalt, or if within the forested buffer the trail may be a forest trail of gravel or compacted dirt.

B. Future Sub Area Planning

Applicants representing ownership of the full 20.2 acres contained in the East Edge may work actively with the City to create amendments or revisions to these development standards provided the amendments meet City standards for Comprehensive Plan consistency and public participation as consistent with other City Sub Area Plans.

C. Outright Permitted Uses

BMC Section 20.70.020 shall apply, except that 20.70.020 (u) is modified to read "Residential uses of all types".

D. Prohibited Uses

Section 20.70.050 shall apply.

E. Residential Development

The following standards shall apply to residential developments in the East Edge. Residential development may combine with commercial development in a mixed use development.

- (1) **Maximum Density:** 20 units per acre of gross site area before exclusion of space for Forest Buffer, public features or other critical areas.
- (2) **Maximum Height:** 55'
- (3) **Building Setbacks:** None, except that a 15' minimum setback shall be maintained where a lot line is shared directly with a lot in the R-10 zone.
- (4) **Development Coverage:** No maximum provided landscaping, design and other standards are met.

- (5) **Building Coverage:** Non maximum provided landscaping, design and other standards are met.

F. Commercial Development

The following standards shall apply to commercial developments in the East Edge. Commercial development may combine with residential development in a mixed use development.

- (1) **Maximum Height:** 55'
- (2) **Building Setbacks:** None, except that a 15' minimum setback shall be maintained where a lot line is shared directly with a lot in the R-10 zone.
- (3) **Development Coverage:** No maximum provided landscaping, design and other standards are met.
- (4) **Building Coverage:** No maximum provided landscaping, design and other standards are met.

G. Design Standards

New development must include design features to contribute the overall unity and vitality of the Center. A building must have at least one designated Front Façade that meets the full range of design standards befitting of a Center. All other building facades must meet standards for Secondary Facades that also contribute to the Center environment and pedestrian experience, but are not as stringent. Any building may be designed with multiple Front Facades.

- (1) **Front Façades:**
 - a. **Window Fenestration:** For ground floor commercial uses, large windows with transparency allowing views into interior spaces or onto displays must comprise 65% or more of the façade between the height of 2' and 10' above sidewalk grade.
 - b. **Ground Floor Residential:** For ground floor residential uses, ground-related housing units are required, and an entry feature to each ground related unit such as a porch or stoop must be included.
 - c. **Building Entrances:** Commercial building entries shall be easily identifiable from the street and sidewalk. This may be accomplished by: a recess or protrusion of at least 3'; or a significant architectural detail or feature around the door.
 - d. **Awnings and Canopies:** For commercial structures an overhang awning or canopy shall be provided for at least 40% of the building's Front Façade width between the height of 8' and 20' above grade.
- (2) **Secondary Facades:**
 - a. **Blank Facades:** No Secondary Façade may be blank. Secondary facades must be variegated with a combination of changes in plane, changes in color and material, windows or architectural features, decorative signage, or other devices to add interest. Variegating features must be included at horizontal or vertical intervals not exceeding 20 feet.
 - b. **Landscape Screening:** Landscape screening must be placed at the base of all secondary facades per the landscape requirements below.

H. Parking

- (1) **Parking Quantity Residential:** The minimum parking requirement for residential uses is one-half off street parking space per 1,000 square feet gross floor area.
- (2) **Commercial Parking:** Non-residential uses shall provide at least one but not more than 3 off street parking spaces per 1,000 square feet gross floor area.
- (3) **Shared Parking:** The following shared parking techniques may be used to reduce the amount of required parking:
 - a. **Time Shared Parking:** Any parking stall the applicant can demonstrate to be shared by users during complimentary hours, such as residential user after 5PM, and employee user between 8AM and 5PM may count towards the parking requirement for both uses.
 - b. **Satellite Parking:** Required parking may located in any satellite location within the Wheaton – Riddell District Center pursuant to the Satellite Parking requirement per BMC 20.48.220.

I. Landscape Requirements

Landscaping requirements are per BMC Chapter 20.50 with the following exceptions.

- (1) **Street Trees:** One street tree of DBH 2.5 inches each 25' on center shall be planted at the site interior circulator where a proposed development is adjacent to the Site Interior Circulator. Averaging may be used to allow clustering.
- (2) **Secondary Façade Screening:** Secondary facades shall be landscaped as follows.
 - a. **Landscaped Area:** A landscaped area at least 5' in width at the base of the secondary façade shall be planted with shrubs, flowers, natural grasses, ferns, hedges or similar thick vegetation. Vegetation must cover at least 80% of the area.
 - b. **Planting Screen:** A thick vegetative sight barrier a minimum of 5' high shall be located within the landscaped area for the width of the secondary façade.
- (3) **Residential Setback Screening:** Within the required setback from lot lines shared by the R-10 zone a planted landscaped area not less than 10' in width shall be provided. At least 80% of the area shall be planted with dense vegetation. Within the planted area trees of DBH 2.5 inches each shall be 25' on center. Preservation of existing trees and vegetation within the setback may substitute for these requirements.

J. Site Interior Circulator

New construction must not preclude the creation of a principal north / south site interior circulation road. The site interior circulator roadway shall be located in the general vicinity as indicated on the circulation figure (Section II) of this Sub Area Plan.

- (1) **Cartway:** The site-interior circulator shall provide a vehicle cartway not less than 24' wide. The City may perform site plan review to ensure that location of buildings do not preclude the creation of a site-interior circulator. The site-interior circulator shall enable connections to adjacent parcels and/or roads.
- (2) **Continuous Walking Surface:** With new development, one continuous north / south walking surface shall be provided at both the Mid-Site Circulator, and the Interior Circulator. The continuous walking surface is not required to be immediately adjacent to the circulator if it is provided at the base of a building façade. The continuous walking surface may be concrete or decorative paver materials, but may not be plain asphalt.
- (3) **Hollis St. Alley:** Require new development to maintain access to the alley at the rear of the homes fronting on Hollis St. via Maple Ave. Infill development may not be cited to impede adjacent residents' use of the alley, provided the alley does not encroach onto the east edge parcels.

K. Ownership & Construction Standards

The Site Interior Circulator may initially be located on private property. The following construction and maintenance standards apply in addition to those of the BMC engineering division.

- (1) **Construction:** The Site Interior Circulator is not required to be constructed to standards of a City roadway. The surfaces of the circulator must be constructed to the standard of a commercial parking lot.
- (2) **Striping:** Striping of the mid-site circulator shall be per the methods and standards of a City roadway. Striping shall demarcate clear circulation and shall be of a readily visible and durable material. The City may require periodic maintenance of the striping to ensure safe and efficient circulation.

L. Signage

Standards of BMC Chapter 20.52 shall apply as follows:

- (1) Downtown and Centers Sign Regulations per (BMC 20.52.140) shall apply.

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